



European Road Safety Charter

Commitment Guidelines

Focus on youth
commitment

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1 The renewed European Road Safety Charter

The European Road Safety Charter (ERSCharter) is a civil society platform on road safety based on concrete commitments undertaken by businesses, associations, local authorities, research institutions, universities and schools. To date, more than 2,300 entities from all EU Member States have endorsed the ERSCharter with commitments aiming to improve the road safety standards and to develop a road safety culture among European citizens. The new phase of the ERSCharter 2013 – 2016 has evolved from the experiences and lessons learnt from the two previous editions. The principle underlying the ERSCharter remains the same: that ownership and responsibility for road safety should be shared with all the stakeholders, both experts and non-experts. The new slogan speaks for itself:

**Together we are
road safety**



TAKE ACTION

2 European Road Safety Charter commitment

2.1 Who are these guidelines for and why?

The ERSCharter is open to all types of organisations (such as businesses, associations, local authorities, research institutions, universities and schools) who wish to make a difference in road safety within the scope of their activity.

These guidelines aim to assist the ERSCharter members to elaborate effective road safety commitments by providing them with necessary knowledge and recommendations on how to plan and evaluate their actions.

They build on knowledge gathered from over 2,300 commitments designed and implemented by the ERSCharter members during 2005 – 2013.

A special focus is placed on commitments and preventive actions that work with youth.

2.2 ERSCharter principles

In order to become part of the ERSCharter, each member must subscribe to the general principles, highlighting:

- Their general commitment to road safety
- Their willingness to share knowledge and practices
- Their sense of ethics

These principles, redesigned for the third phase of the campaign, lie at the heart of the ERSCharter commitment.

All organisations subscribing to these principles become members of the ERSCharter and are invited to develop their commitment and share their actions and activities with the rest of the community and society at large.

The principles of the ERSCharter

We choose to subscribe to the following ERSCharter principles:

1	We acknowledge the importance of developing a positive road safety culture in the European Union to progress towards our 'vision zero' for road safety and halve road casualties.
2	We recognise the importance of taking part in a European initiative that gathers members from all Member States and includes organisations of all types* and sizes, with or without previous experience in road safety.
3	We believe in the need for our organisation to take or support action for road safety in our field of expertise.
4	We voluntarily choose the action that best suits our needs, capacities and skills.
5	We support the exchange of information and experiences between ERSCharter members to generate new knowledge on road safety.
6	We believe in communicating openly, honestly and accurately with one another.
7	We honour the rights, beliefs and cultures of every member and every individual, and will treat one another with the highest degree of respect and dignity.
8	We will enjoy the same rights as all other members to access and disseminate information, participate in the ERSCharter activities and use the ERSCharter visual identity.
9	We understand that the ERSCharter adheres to strict ethical rules to avoid any misuse of membership.

2.3 Going beyond

All ERSCharter members have the option to develop a more complex commitment with impact assessment, which can become a candidate for Good Practice. Any member can decide to deepen the scope of their commitment at any point during the duration of the project. Please consult our [Candidates for Good Practice Guidelines](#) or contact the ERSCharter team for more information.

2.4 What will I find in these guidelines?

These guidelines will provide information regarding:

- How to define your objectives?
- How to prepare an action plan?
- Why is evaluation important?
- How to design your evaluation plan?
- How to complete you application on the ERSCharter website?
- How to get further information on your commitment?



3 Making a ERSCharter commitment

3.1 How to define your objectives?

Your commitment is a concrete result of your motivation and interest for improving road safety standards in your environment. The first step to design your ERSCharter commitment is to identify which problem you want to tackle. In order to do this, the following questions can help you:

- What road safety problems do I identify in my environment?
- How do I want to respond to these problems?
- How can I do it within my financial and organisational possibilities?
- You can think outside the box and be innovative

See how others did it:

Testimonial from Chaves-Verín¹

–What are the main actions you have undertaken since you signed?

"Given traffic volumes at the border, especially at weekends, and the high number of accidents, we need common solutions for victims and to improve safety in border regions.

We've removed hazardous road architecture, maintained and replaced traffic signals and carried out joint safety and driver education activities and traffic controls involving Spanish and Portuguese police."

Testimonial from Multipack²

–What are the main actions you have undertaken since you signed the Charter?

"We produced and distributed 4 million white sugar sticks carrying the motto 'Let's save 25 000 lives' and 10 different road safety slogans, as well as 100 000 examples of our own products carrying stickers with the same motto and the slogan 'Thank you for driving safely'. We also published road safety material at our website and Facebook page.

We believe that the proximity of our products to consumers has made them think about what they can do to help cut road accidents."

¹ Verín, in Spain, and Chaves, in Portugal, are towns close to the border between the two countries. They jointly signed the Charter in April 2010.

² Multipack Bulgaria produces packaging for products including sugar, creamer, instant coffee, sweetener, honey, hot drink mixes, salt and pepper. It signed the Charter in February 2011.

3 Making a ERSCharter commitment

Once you identified the road safety problem to improve, you shall start setting your SMART objectives:

Specific

Detail exactly what action you want to carry out, who will benefit from your actions and who will help you to implement it:

- What specifically do you want to do?
- What specifically do I want to achieve?
- Who is my target group?

Mesurable

Your achievements and results can be measured.

- How can I obtain objective knowledge on road safety improvement indicators such as decrease in number of accidents, decrease in risk exposure or behavioural change related to road traffic situations?
- Can I obtain information from the participants related to the effect and scope of the actions?
- How many people will benefit from my actions?

Achievable

Be realistic and set objectives that are aligned with your financial and organisational resources and possibilities.

- Who in my organisation will be implicated in the development of the actions?
- What activities will I implement?
- What resources do I have to develop my actions?

Realistic

Your objectives should clearly respond to a road safety problem you have identified.

- Will my action have a real impact on improving road safety or is it merely 'folkloric'?

Timed

You should define the timeline for your actions: a lack of deadlines is the best excuse for not doing anything!

- Does my action have clear deadlines?

3 Making a ERSCharter commitment

3.2 How to prepare your action plan in 10 steps?

	Step	Ask yourself	Write down
1	Problem	Why do I need to act in road safety? What are the concrete problems to tackle?	Relevant statistical data that illustrate your road safety problem
2	Commitment	What is my action about? What are the relevant indicators for its success?	Indicators of success
3	Knowledge	What do I know about to road safety problem I identified? What kind of expert knowledge do I need?	Aspects related with your problem and source of expert knowledge
4	Target group	Who will my action be targeted at? Who will benefit from my action?	People affected by the problem
5	Experts	Who will be most indicated to implement my action?	Experts who will collaborate
6	Activities	What concrete activities will I carry out?	Actions that could produce results
7	Methods and tools	What methods (e.g. training) and tools (e.g. leaflets) will I use?	Tools to be designed and used in your action
8	Implementation and teams	How do I plan the implementation process? Who from my organisation will participate?	Action plan and team chart
9	Expected results	What results do I expect?	Expected results
10	Communication	How will I communicate on my results?	Means and communication plan

3.3 Targeting youth

Youth constitutes a key priority for the European Road Safety Charter in the years to come.

Road traffic injuries are the leading cause of death among people under 24 years old. Age groups that have the highest percentage of pedestrian fatalities are children younger than 10 years (and adults aged 65 years or older). In addition, cyclist fatalities have the highest share among children between 6 and 14 years of age. Young people aged between 15 and 24 account for just 13% of the population in Europe, yet they still make up 25% of the deaths on the road.

Why are young people especially at risk?

- Patterns of mobility: young people start being car/motorbike drivers
- Inexperience and lack of skills (such as driving) and knowledge about road safety
- Experimentation with alcohol and drugs
- Lack of parent/adult supervision
- Over confidence
- Higher exposure to peer pressure
- Tendency to risky behaviour as natural part of developmental process

Thus, the ERSCharter especially encourages members to develop initiatives targeting youth. Youth actions can address one or more of the following age groups:

0 – 11 Children and infants
12 – 14 Pre-teens
15 – 17 Adolescents
18 – 25 Young adults



3 Making a ERSCharter commitment

Here you will find some examples of actions towards youth implemented by the ERSCharter signatories, classified in three categories: Education & Enforcement, Vehicle & Personal Safety, and Infrastructure Safety & Post-crash Care:

Pre-teens and Adolescents

Education & Enforcement

[A Maiuri Secondary School](#), Pompei (Italy) organises an awareness raising campaign for road safety and develops training for novice moped drivers.

[FEDEMOT](#) (Belgium) organises road safety days in schools to improve the road safety of motorised two-wheelers, with different workshops including first aid training.

[Maastricht University](#) carries out a research project on traffic behaviour among teenagers (cycling), adolescents (riding mopeds), and older adolescents (driving cars).

[The Latvian Medical Students' Association](#) is planning to prepare lectures for teenaged pupils about road safety, including drink driving (also on bicycles) and safe pedestrian and cyclist behaviour.

Vehicle & Personal Safety

[McRf](#) has committed to developing a campaign to counteract tampering through the Swedish moped dealers.

[Pirelli](#) is developing a campaign, with a safety message including advice on how to enjoy driving motorcycles in a safe way (homologated helmet, protective clothing, motorcycle integrity and respect of the highway code).

Infrastructure Safety & Post-crash Care

[Red Cross Slovakia](#) offers first aid lectures to students and driving license applicants.



Young adults

Education & Enforcement

[Mid and West Wales Fire and Rescue Service](#) runs the Pass Plus Cymru Scheme aiming at young drivers (17-25) who have recently passed their test and aims to help them improve driving skills, gain extra experience, and possibly obtain a discount on their car insurance.

[The Hellenic Association of Insurance Companies](#) will organise night operations with distribution of leaflets to drivers with a powerful message against drink driving.

[Driving Research Group, Cranfield University](#) will work in partnership with a²om, the world's first academic qualification in novice driver training. They will assist in the design and development of technical solutions for educating drivers about the driving risks using the Goals for Driver Education as an underpinning foundation.

[FAGE](#) is developing a broad awareness campaign about drink-driving and providing tools and training to its members, the students' association, to organise safe events.

[The State Health Centre \(LZG\)](#) is a collection of more than 50 establishments and associations in Bavaria that work in the areas of health and prevention. Their 'Disco Fever Project' encourages adolescents and young adults to behave responsibly on the road through an emotional speech on a level that will enable personal engagement with the subject.

Vehicle & Personal Safety

[Dekra e.V.](#) is offering safety checks for vehicles of young drivers (18–24 years old).

Infrastructure Safety & Post-crash Care

[FECASARM, the Catalan Federation of Associations of Leisure Businesses](#), will promote the use of night bus routes: its members will give non-alcoholic drinks and bar or nightclub admission discounts to those who show their bus tickets.

[STIVO](#) (France) has designed in coordination with students' unions, the 'Student shuttle buses' aimed at bringing young people to and from student parties and events in the utmost safety. Student unions are also committed by means of signing the responsible driving charter.

4 Carrying out the evaluation

4.1 Why is evaluation important?

Although all commitments and actions may contribute to improving road safety standards, it is important to assess whether your action is being developed according to the initial plan and meeting objectives for the following reasons:

- It can help you to improve your actions and to identify what works and what does not work so that you can improve it
- It can help you to measure the impact of your actions and to what extent you have achieved your goals
- The exact knowledge about the results of your actions is necessary to make investment decisions and decide on the future of your actions
- You will gain objective knowledge on the effectiveness of your actions that can guide and inspire other ERSCharter members, shape and guide your organisation's future decisions or even influence road safety policy
- Inclusion of the evaluation in your action plan doesn't need to be necessarily expensive; on the contrary, not being aware of your achievements and mistakes may be much more costly



4.2 How to define your indicators?

The first step is to define the indicators against which your action will be measured. This need not be complicated if you have properly set up your SMART goals and developed the action plan.

Below we include a list of indicators that could be used to measure the effectiveness of road safety actions.

Objective indicators

- Decrease in the number of accidents and victims
- Decrease in the number of traffic safety offenses, especially the most serious ones (seat belt use, speeding, alcohol and drug risk behaviours, etc.)
- Decrease in material and time related costs of accidents
- Decrease in the level of risk exposure: number of trips, types or length of trips, increased use of public transport, etc.
- Indicators measuring participation: number of participants and beneficiaries of your actions
- Increased visibility of your actions in social media or other communication means
- Decrease in levels of air pollutants emission related to the use or production of vehicles

Subjective indicators based on participants' or beneficiaries' insights and subjective perceptions:

- Increase of road safety knowledge, as manifested by:
 - Positive changes in attitudes related to road safety behaviours
 - Positive change in road safety habits
- Indicators based on perception of external observers or road safety programme managers/implementers:
 - Evaluation of the degree of compliance with the goals and objectives of your actions
 - Evaluation of the degree of compliance with the initial action plan
 - Evaluation of the relevance of the actions
 - Evaluation of the communication efforts or presence on the social media

Not all indicators are equally valid or important. There is no doubt about the final goal to reduce the number of accidents and their consequences, however its achievement requires time, means and joint commitment of all sectors of the society: ***Together, we are Road Safety!***

Expert tip

Choose indicators that are in the scope of your financial and organisational possibilities. If you want to go further and make a commitment with a more complex evaluation plan, consult our Candidates for Good Practice Guideline or the ERSCharter team.

4.3 How to design the evaluation plan?

The evaluation has a double function.

Formative evaluation

In this case, projects or actions are typically assessed during their development or early implementation phases to provide information about how best to revise and modify for improvement. It responds to questions such as: Is the implementation of the activities going according to the plan? How can I improve my activities?

Formative evaluation is about **products** and concrete **processes** of the implementation. It generally applies to each activity of your action plan.

Summative evaluation

In summative evaluation, programmes or projects are assessed at the end of an operating cycle, aiming to try and understand the effects of the actions. It responds to questions such as: Is my action producing the effect according to the objectives I identified?

Summative evaluation is about the **outcome** and **impact**. In general, this kind of evaluation is planned previously to the implementation of the activities but is carried out at the end of the whole implementation process.

Both types of evaluation can naturally be combined: summative evaluation will indicate if you have achieved your goals, while formative evaluation will inform you about your achievements throughout the implementation of your action and how they can be improved.



Here is an example of the type of information you may obtain from an evaluation:

☐ Evaluation of activities (activity sheet):

Describe your activity:

Type of activity? (Course or training activity, Improvement, new design, new incorporation, Communication, dissemination, campaign, media, Legislative change...)

Degree of compliance with the initial plan:
Degree of satisfaction of the participants/beneficiaries with the action:
Degree of satisfaction of the organisers and managers with the action:

Indicators

Number of participants:

Number of beneficiaries reached:

Number of material produced:

Expert tip

To examine whether your action has produced significant effect on the indicators, you have to plan and carry out an impact assessment. We invite you to read the Candidate for Good Practice Guidelines which we elaborated for this effect.

Learnings

What lessons have I learned?

What are my conclusions from the action?

How can I replicate/scale up the action in the future?

5 Your commitment on the ERSCharter website

In the renewed ERSCharter we wish to focus on sharing road safety practice among the platform members and beyond. Our website was redesigned for the 3rd edition with the aim of giving you more opportunities to comment, exchange information, share experiences and get inspiration from others.



5 Your commitment on the ERSCharter website

What you can do in the **My Charter** section of the ERSCharter website?

The screenshot shows the 'My Charter' section of the ERSCharter website. The page is titled 'MOBILITY AND TRANSPORT' and 'European Road Safety Charter'. It includes a navigation bar with links like 'Road Safety in action', 'Resources & knowledge', 'The Charter across Europe', and 'About the Charter'. The main content area is divided into several sections:

- 1 My activity:** A form to create a new activity. It includes a 'Description' field, 'Add image', 'Add video', 'Add document', and 'Add link' buttons. There are also dropdown menus for 'Topic', 'Tag', and 'Who'. A checkbox 'This is a PUBLIC EVENT. I want people to attend' is present, along with a 'Publish' button.
- 2 Tell the world how you are contributing to road safety in Europe?** A large blue banner with a white text overlay.
- 3 My Recent Activity:** A list of recent activities. Each entry shows a date (e.g., '1 August 2013'), a location (e.g., 'Toulouse Motoring France'), and a title (e.g., 'Activity Title (link)'). It also includes social media icons and a count of documents and photos.
- 4 CALL FOR GOOD PRACTICE:** A blue box with white text asking if an organization's activity qualifies as a Charter good practice, with a 'Read more' link.
- 5 TELL US YOUR CHARTER EXPERIENCE:** An orange box with white text asking for a testimonial.
- 6 Your Customized Toolkit:** A section with three white boxes labeled 'Customized web banner', 'Graphical guide', and 'Promotional kit'.
- 7 Get Inspired – Activities from similar members:** A list of activities from other members, similar to the 'My Recent Activity' section.

- 1 You can share information about your road safety activities.
- 2 You can store information about your previous commitments (for previous signatories).
- 3 You can share information about your road safety events, make them public and invite participants
- 4 You can inspire others by sharing a testimonial about any aspect of your road safety commitment and the process of developing and implementing of your actions.
- 5 You can access your customised toolkit for promotional material and commitment guidelines.
- 6 You can get help to elaborate a more complex commitment with impact assessment (call for Good Practice).
- 7 You can see similar actions of other ERSCharter members

These guidelines have been developed by P.A.U. Education in collaboration with European Road Safety Charter experts' team coordinated by Dr. Jaime Sanmartín, as part of the contract placed by European Commission, Directorate General for Mobility and Transport.

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